

# OLD SOUTH AVENUE REDEVELOPMENT PLAN

BOROUGH OF FANWOOD, NEW JERSEY



Prepared for the Borough of Fanwood

by **PHILLIPS PREISS GRYGIEL LEHENY HUGHES LLC** | Planning and Real Estate Consultants | Adopted August 5, 2019

**Old South Avenue Redevelopment Plan**  
**Borough of Fanwood, New Jersey**

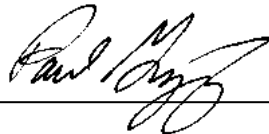
Adopted August 5, 2019

**Prepared for:**  
Borough of Fanwood

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The original of this report was signed and sealed in accordance with N.J.S.A. 13:41-1.2



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# 1 Introduction

## A. BASIS FOR THE PLAN

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This redevelopment plan has been prepared for an area consisting of multiple properties located along South Avenue and Old South Avenue within the Borough of Fanwood, Union County, New Jersey (the “redevelopment area”). The redevelopment area is comprised of nine tax lots located on the northwest side of South Avenue and Old South Avenue in the vicinity of Stagaard Place known as Block 55, Lots 7.01, 8, 9.0101, 9.0102, 10, 11, 11.01 and 12 and Block 57, Lot 1, as well as the right-of-way of Old South Avenue.

On August 6, 2018 the Fanwood Borough Council adopted Resolution 18-08-135 authorizing the Fanwood Planning Board to conduct a preliminary investigation and to prepare a report pursuant to N.J.S.A. 40A:12A-6 to determine whether the properties in the redevelopment area qualify as an “area in need of redevelopment” in accordance with the criteria specified in the New Jersey Local Redevelopment and Housing Law (LRHL) at N.J.S.A. 40A:12A-5. Phillips Preiss Grygiel Leheny Hughes LLC subsequently conducted a study of these properties on behalf of the Planning Board. The Board held a public hearing on September 26, 2018 to hear planning testimony on the condition of the properties. At the conclusion of the hearing, the Planning Board voted in favor of adopting the findings of the redevelopment study. The Fanwood Borough Council subsequently approved the Planning Board’s determination and declared the area a “Non-Condemnation Redevelopment Area” pursuant to N.J.S.A. 40A:12A-6.

The LRHL at N.J.S.A. 40A: 12A-7 provides that no redevelopment projects shall be undertaken or carried out except in accordance with a redevelopment plan adopted by ordinance of the municipal governing body upon its finding that the specifically delineated project area is located in an area in need of redevelopment or an area in need of rehabilitation, or in both, as appropriate. This redevelopment plan provides a framework for the redevelopment of the redevelopment area.

## B. PURPOSE AND VISION

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This Redevelopment Plan sets forth standards for the construction of buildings and other improvements in the redevelopment area. The purpose of this Redevelopment Plan is to provide a framework for the potential redevelopment of properties in the redevelopment area in accordance with smart growth principles and current design techniques. The Borough of Fanwood has been proactive in planning for the redevelopment of its downtown in the vicinity of the Fanwood train station, resulting in attractive, mixed-use, transit-oriented development. Its planning efforts have also addressed the South Avenue corridor outside of the downtown.

It is the vision of this Redevelopment Plan to improve a portion of the South Avenue corridor by promoting a mix of land uses appropriate for a non-downtown setting, including multifamily residential and limited commercial uses, with an emphasis on high-quality building, site and landscape design. Open space and other amenities must be provided for larger development projects within the rede-

velopment area. A focus of this plan is to improve mobility in this area for pedestrians and bicyclists, as well as motorists. Development in this area will be consistent with other planning and implementation projects, notably the Borough's Vision Plan for the South Avenue corridor and the recently created Commercial Corridor zone districts, as well as the Transportation Alternatives Program (TAP) grant from the New Jersey Department of Transportation (NJDOT) and the North Jersey Transportation Planning Authority (NJTPA). The TAP grant will be used to enhance the streetscape and non-motorized transportation facilities along South Avenue in Fanwood, including abutting the Study Area. The vacation of Old South Avenue in the redevelopment area will help achieve this vision.

### **C. NOTES ON PLAN TERMINOLOGY**

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Throughout this Redevelopment Plan, a conscious distinction is made in the regulations between "shall" and "should."

- "Shall" means that a redeveloper is required to comply with the specific regulation, without any deviations, other than those deviations that may be permitted by the Planning Board, as hereinafter provided.
- "Should" means that a redeveloper is encouraged to comply but is not required to do so.

### **D. DEFINITIONS**

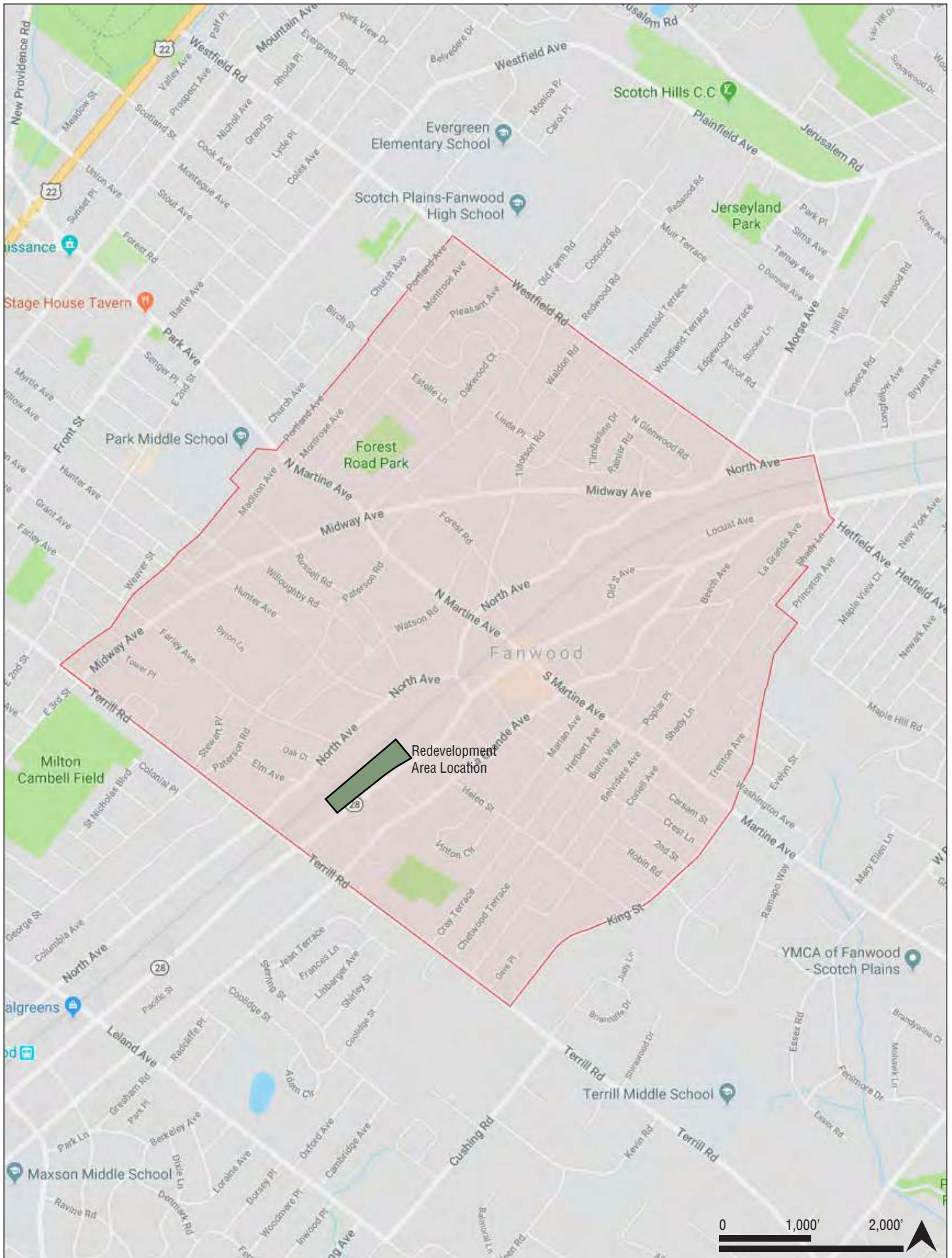
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The definitions set forth in the Land Use Ordinance of the Borough of Fanwood shall apply to this Redevelopment Plan. If a term used in this Redevelopment Plan is not defined in the Land Use Ordinance, the definition in the Municipal Land Use Law or the Local Redevelopment and Housing Law shall apply.

## 2 Context

The redevelopment area consists of developed and undeveloped lands located in the southwestern section of the Borough of Fanwood known as the Old South Avenue Corridor. The Study Area includes Block 55, Lots 7.01, 8, 9.0101, 9.0102, 10, 11, 11.01 and 12 and Block 57, Lot 1, as identified on the official tax maps of the Borough of Fanwood, as well as any or all streets and rights-of-way, which in this case includes the right-of-way of Old South Avenue. Its locational context within the Borough of Fanwood is shown on Figure 1. Figure 2 shows the boundaries of the redevelopment area on the Borough's tax maps. Figure 3 shows the redevelopment area depicted in an aerial photograph.

Land uses within the vicinity of the redevelopment area are varied and include a mix of residential and nonresidential uses. A preschool/childcare center is located to the northeast. To the southeast are residences across South Avenue as well as commercial uses, including a liquor store on a long, shallow lot between Old South Avenue and South Avenue. A mattress factory is located to the southwest. Residential uses are located to the northwest across the railroad tracks. Other land uses in the vicinity include office, light industrial, pet daycare, an animal hospital, a funeral home, commercial recreation and public utility facilities. Although South Avenue is a New Jersey State Highway designated as Route 28, it is not a divided highway but instead a two-lane road more akin to a local street. The site also is not located proximate to any such major highways.



**Figure 1: Redevelopment Area Location** Old South Avenue Redevelopment Plan, Borough of Fanwood, New Jersey  
 PHILLIPS PREISS GRYGIEL LEHENY HUGHES LLC 2019





**Figure 2: Redevelopment Area Boundaries** | Old South Avenue Redevelopment Plan, Borough of Fanwood, New Jersey  
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11 Redevelopment Area Lots



**Figure 3: Redevelopment Area Context** | Old South Avenue Redevelopment Plan, Borough of Fanwood, New Jersey  
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# 3 Goals of the Redevelopment Plan and Relationship to Local Objectives

## **A. REDEVELOPMENT PLAN GOALS AND OBJECTIVES**

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The specific goals and objectives of the Redevelopment Plan are as follows:

- 1) Create a visually stimulating and economically viable mixed-use area
- 2) Accommodate vehicles, pedestrians, bicyclists and transit users
- 3) Stimulate pedestrian use of the corridor in the redevelopment area
- 4) Provide facilities for bicyclists in the redevelopment area
- 5) Develop attractive buildings and appealing site design
- 6) Minimize visibility of parking lots and automobile accommodations
- 7) Build upon other planning and implementation projects for the redevelopment area and vicinity, including the Vision Plan for the South Avenue corridor, the Transportation Alternatives Program (TAP) grant and the Commercial Corridor zone districts

## **B. RELATIONSHIP TO MASTER PLAN**

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The Borough of Fanwood adopted its most recent Master Plan Reexamination Report in 2010. The 2010 Reexamination states Fanwood is “committed to the future development and revitalization of the South Avenue corridor.” Within the section of the 2010 Reexamination that recommends specific changes for the Borough’s Master Plan and development regulations, there is specific discussion with regard to the South Avenue corridor. It is noted on page 20 of the 2010 Reexamination that “(t)he Planning Board recommends the western portion of the South Avenue corridor be considered for zoning changes” in accordance with recommendations of the 2007 Vision Plan for this area. Notably, it is recommended that the redevelopment area and vicinity move away from the light industrial zoning in place in 2010 to allow for a greater mix of uses, with a list of permitted uses that is “more expansive than those permitted in the downtown redevelopment area.” A three-story limit on building height is suggested in the 2010 Reexamination, which is proposed for smaller properties in the redevelopment area. However, this Redevelopment Plan permits increased building height on larger properties, and only if the appearance of the top story is minimized through architectural design.

The South Avenue corridor is identified as an issue in the 2004 Master Plan Reexamination as well, which also includes a detailed discussion of the corridor as well as recommendations for zoning changes. The 2004 Reexamination notes that South Avenue “serves as one of the primary gateways

into the Borough's downtown" (page 7). It also states on page 6 that the light industrial zone on the portion of the South Avenue corridor that includes the Study Area requires "a new land use vision due to its transition from primarily an industrial district to a district with a mixture of uses," with improved bulk standards. This Redevelopment Plan is consistent with these recommendations and supports the creation of an attractive gateway to Fanwood and its downtown.

### **C. RELATIONSHIP TO ZONING ORDINANCE**

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The redevelopment area is located within the Commercial Corridor Western (CC-W) District. The CC-W zone is one of three recently created Commercial Corridor zone districts in Fanwood, which cover a number of properties along the South Avenue corridor.

The CC-W and other CC districts have somewhat varied use and bulk regulations that relate to their land use contexts. A wide range of principal uses are permitted in the CC-W zone, including retail and service commercial, civic, institutional, artisanal and residential uses, including multiple uses and/or principal buildings on a single lot. The list of permitted uses in this Redevelopment Plan is somewhat more focused than in the CC-W zone overall, in order to promote more attractive, pedestrian-friendly development. The closing of Old South Avenue as recommended by this plan will bolster the change in character to a mix multifamily residential and limited commercial uses envisioned by this Redevelopment Plan.

The Commercial Corridor zone regulations in Section 184-119 of the Land Use Ordinance of the Borough of Fanwood shall apply to this redevelopment area, except as noted in the following chapter. Where there is a conflict between the requirements of this Redevelopment Plan and the Land Use Ordinance, this Redevelopment Plan shall prevail. Final adoption of this Plan by the Borough Council shall be considered an amendment of the Borough of Fanwood Zoning Map.

## 4 Use, Bulk and Design Regulations

### A. LAND USES

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Permitted uses shall be as listed in Subsection 184-119B of the Land Use Ordinance of the Borough of Fanwood, except that uses in the category of “Artisanal / Light Industrial / Other” shall not be permitted in new buildings in the redevelopment area.

### B. BULK REGULATIONS

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Bulk standards shall be as listed in Subsection 184-119C of the Land Use Ordinance of the Borough of Fanwood, with the following exceptions:

- Subsection 184-119C(2)(b): In the event Old South Avenue is vacated, the maximum front setback shall not apply to a building that existed at the time the street was vacated and had its sole frontage on Old South Avenue. Any new building constructed in the redevelopment area subsequent to the vacation of Old South Avenue shall comply with this standard.
- Subsection 184-119C(2)(e): On any property with a lot area of 60,000 or greater, the maximum permitted building height shall be increased to 4 stories. The fourth story shall be under a pitched roof or have a pitched accent roof along its facades, and shall contain dormers or other architectural features to give the building the appearance of having only 3.5 stories. The maximum height of a 4 story building shall be 50 feet to the flat portion of the roof and 56 feet to the highest point of the pitched roof or pitched accent roof, as measured from the mean elevation of the grade plane from the original lot grade or any revised lot grade shown on a site plan, subdivision plan or plot plan approved by the Planning Board along the South Avenue frontage of the building.

### C. BUILDING, SITE AND STREETScape DESIGN STANDARDS

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Architectural, streetscape and other design standards shall be as listed in Subsections 184-119D through I of the Land Use Ordinance of the Borough of Fanwood, with the following exceptions:

- Subsection 184-119D(4): The vertical building wall offset standards shall apply. In addition, the maximum horizontal distance between such vertical offsets shall be thirty (30) feet.
- Subsection 184-119E(1)(b): The pedestrian clear zone shall be a minimum of ten (10) feet within the redevelopment area.
- Subsection 184-119E(2): Sidewalks shall be a minimum of ten (10) feet wide along South Avenue within the redevelopment area.

Subsections 184-119D through I of the Land Use Ordinance of the Borough of Fanwood are appended to this Redevelopment Plan for reference.

The use of green infrastructure in site and streetscape design to mitigate storm water management and quality is encouraged, when possible.

#### **D. OPEN SPACE AND AMENITIES**

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Open space shall be provided as part of development on any property with a lot area of 60,000 or greater. This open space may include areas at grade level as well as on rooftops. Rooftop open space areas shall comply with the following standards:

- Rooftop space may be open to the public, or restricted to use by residents, visitors and/or patrons of a business.
- Rooftop amenity areas shall be designed in an attractive manner that will not detract from adjacent uses, and will prevent nuisance and safety issues. Design of the rooftop must utilize all reasonable efforts to minimize the effect of noise, light, and odor on adjacent properties and on the street.
- Parapet walls or guardrails shall be designed to complement the building's architectural design and shall meet all applicable construction codes.
- Pergolas and temporary tents, umbrellas and/or awnings or other temporary structures shall be permitted on the rooftop provided evidence of how such structures shall be safely secured to the rooftop is presented to the Planning Board.
- Lighting shall comply with § 184-101 of the Fanwood Land Use Ordinance, however, due to the rooftop location, lighting may be located greater than 15 feet above normal grade, and the use of lights spaced evenly along a cable or wire, commonly known as "string lights," shall be permitted.

An example of a rooftop open space area for illustrative purposes only is shown in the photographs on the following page.



*Above: examples of rooftop open space from Cranford, NJ*

## 5 Plan Consistency Review

### **A. RELATIONSHIP TO MASTER PLANS OF ADJACENT MUNICIPALITIES**

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The redevelopment area is situated in the interior of the Borough of Fanwood. It is not located adjacent to any other municipalities. The permitted land uses and other development regulations in this redevelopment plan are similar to the existing zoning for the redevelopment area properties. Therefore the redevelopment plan would not impact the master plan of any municipality adjoining Fanwood.

### **B. RELATIONSHIP TO THE UNION COUNTY MASTER PLAN**

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The Redevelopment Plan is consistent with the Union County Master Plan, as this plan directs growth to environmentally suitable areas that have existing infrastructure and supporting facilities. The Redevelopment Plan also promotes new commercial and housing development in an appropriate location, thus furthering the County Master Plan's goals for housing, development, transportation and economic development.

### **C. RELATIONSHIP TO THE STATE DEVELOPMENT AND REDEVELOPMENT PLAN**

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The New Jersey State Development and Redevelopment Plan (SDRP) was originally adopted in 1992. The purpose of the SDRP according to the State Planning Act at N.J.S.A. 52:18A-200(f) is to:

“Coordinate planning activities and establish Statewide planning objectives in the following areas: land use, housing, economic development, transportation, natural resource conservation, agriculture and farmland retention, recreation, urban and suburban redevelopment, historic preservation, public facilities and services, and intergovernmental coordination.”

A revised version of the plan was adopted by the State Planning Commission in 2001. While required by the State Planning Act to be revised and re-adopted every three years, the SDRP has only been re-adopted once during the 27 years since its original adoption. A new State Strategic Plan (SSP) has been proposed as the revision to the 2001 SDRP but has not been adopted as of early 2019.

This Redevelopment Plan is thoroughly consistent with the SDRP and the draft SSP, as it epitomizes the smart growth principles set forth in both documents. In particular, the Redevelopment Plan promotes the reuse of developed property in an area well served by infrastructure and is located proximate to transportation networks. This Redevelopment Plan therefore furthers the goals, strategies and policies of the SDRP and the proposed SSP.



## 6 Redevelopment Actions

The Redevelopment Plan provides for a number of actions in support of the overall Plan goals and objectives, as follows:

### **A. OUTLINE OF PROPOSED ACTIONS**

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Individual property owners and/or redevelopers shall be responsible for redevelopment on their property. The Borough of Fanwood may vacate the right-of-way of Old South Avenue to further the objectives of this redevelopment plan. The disposition of the Old South Avenue right-of-way shall be determined by the Borough of Fanwood.

It is anticipated that mixed-use development with the majority of parking spaces provided in an enclosed garage will be provided on one or more of the larger parcels in the redevelopment area. A conceptual design for this type of development is included in Figure 4.

### **B. PROPERTIES TO BE ACQUIRED**

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This is a “non-condemnation redevelopment area” and thus the Borough of Fanwood will not be acquiring any private properties through this redevelopment process.

### **C. REDEVELOPER’S AGREEMENT**

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Any redeveloper shall enter into an agreement with the Borough no later than 90 days of a memorialized Planning Board resolution of final site plan approval. The Redeveloper’s Agreement shall be in a mutually agreeable form satisfactory to the Borough Attorney and redeveloper.

### **D. OTHER ACTIONS**

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In addition to the construction and other activities described above, several other actions may be taken to further the goals of this Plan. These actions may include, but shall not be limited to:

- Provisions for public infrastructure necessary to service and support new development.
- Environmental remediation.
- Vacation of the right-of-way of Old South Avenue.
- Vacation of public utility easements as may be necessary for redevelopment.
- Any other rights of the municipality as provided in the New Jersey Local Redevelopment and Housing Law at N.J.S.A. 40A:12A-5.



**Figure 4: Conceptual Design of Mixed-Use Development on Lot 12** | Old South Avenue Redevelopment Plan, Borough of Fanwood, New Jersey  
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## 7 General Provisions

### A. SITE PLAN REVIEW

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Prior to commencement of construction on any parcel within the redevelopment area, a site plan for the construction and/or rehabilitation of improvements on the site, prepared in accordance with the requirements of the Municipal Land Use Law (N.J.S.A. 40:55D-1 et seq.), shall be submitted by the applicant for review and approval by the Fanwood Planning Board.

### B. ADVERSE INFLUENCES

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No use or reuse shall be permitted which, when conducted under proper and adequate conditions and safeguards, will produce corrosive, toxic or noxious fumes, glare, electromagnetic disturbance, radiation, smoke, cinders, odors, dust or waste, undue noise or vibration, or other objectionable features so as to be detrimental to the public health, safety or general welfare.

### C. NON-DISCRIMINATION PROVISIONS

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No covenant, agreement, lease, conveyance, or other instrument shall be affected or executed by the Borough Council of the Borough of Fanwood or by a developer or any of his or her successors or assignees, whereby land within the redevelopment area is restricted upon the basis of race, creed, color, ancestry, sex, national origin, family status, disability, or sexual preference in the sale, lease, use or occupancy thereof. Appropriate covenants, running with the land forever, will prohibit such restrictions and shall be included in the disposition instruments. There shall be no restrictions of occupancy or use of any part of the redevelopment area on the basis of race, creed, color, ancestry, sex, national origin, family status, disability, or sexual preference.

### D. DURATION OF THE PLAN

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The provisions of this Redevelopment Plan specifying the redevelopment of the redevelopment area and the requirements and restrictions with respect thereto shall be in effect for a period of not more than 30 years from the date of original adoption by the Borough Council.

### E. DEVIATION REQUESTS

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The Fanwood Planning Board may grant deviations from the regulations contained within this Redevelopment Plan where, by reason of exceptional narrowness, shallowness or shape of a specific piece of property, or by reason of exceptional topographic conditions, pre-existing structures or physical features uniquely affecting a specific piece of property, the strict application of any area, yard, bulk or design objective or regulation adopted pursuant to this Redevelopment Plan, would result in peculiar practical difficulties to, or exceptional and undue hardship upon, the developer of such property. The Fanwood Planning Board may also grant such relief in an application relating to a spe-

cific piece of property where the purposes of this Redevelopment Plan would be advanced by a deviation from the strict requirements of this Plan and the benefits of the deviation would outweigh any detriments. No relief may be granted under the terms of this section unless such deviation or relief can be granted without substantial detriment to the public good and without substantial impairment of the intent and purpose of the Redevelopment Plan. An application for a deviation from the requirements of this Redevelopment Plan shall provide public notice of such application in accord with the requirements of public notice as set forth in N.J.S.A. 40:55D-12a and b.

Notwithstanding the above, any changes to the uses permitted in the Redevelopment area shall be permitted only by means of an amendment of the Redevelopment Plan by the Borough governing body, and only upon a finding that such deviation be would be consistent with and the furtherance of the goals and objectives of this Plan.

## **F. ESCROWS**

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Redevelopers shall be responsible to post sufficient escrows to cover any and all costs of the professional consultants retained by the Borough to review the proposed redevelopment project and advise the Borough on any and all aspects of the redevelopment process.

## 8 Other Provisions

In accordance with N.J.S.A. 40A:12A-1 et seq., known as The Local Redevelopment and Housing Law, the following statements are made:

- The Redevelopment Plan herein has delineated a definite relationship to local objectives as to appropriate land uses, density of population and improved traffic and public transportation, public utilities, recreation and community facilities, and other public improvements. The Plan has laid out various programs and strategies needed to be implemented in order to carry out the objectives of this Plan.
- The Redevelopment Plan lays out the proposed land uses and building requirements for the redevelopment area.
- No displacement and/or relocation of businesses and residents in the redevelopment area is anticipated on the part of the Borough of Fanwood.
- As indicated in Chapter 3, this Redevelopment Plan is substantially consistent with the Master Plan of the Borough of Fanwood. The Plan also complies with the goals and objectives of the New Jersey State Development and Redevelopment Plan and the draft State Strategic Plan.
- As of the date of the adoption of the resolution finding the area to be in need of redevelopment, there were no dwelling units in the redevelopment area. Therefore, the requirement of the Local Redevelopment and Housing Law at N.J.S.A. 40A:12A-7 a.(6) to construct replacement affordable housing units for any that are removed is not applicable.
- If any section, paragraph, division, subdivision, clause or provision of this Redevelopment Plan shall be adjudged by the courts to be invalid, such adjudication shall only apply to the section, paragraph, division, subdivision, clause or provision so judged, and the remainder of this Redevelopment Plan shall be deemed valid and effective.

## 9 Procedure for Amending the Approved Plan

This Redevelopment Plan may be amended from time to time upon compliance with the requirements of state law. Any party requesting a future amendment shall submit such request to the Fanwood Borough Council. The Borough Council may require the party requesting the amendment prepare a study of the impact of such amendments, which study shall be prepared by a professional planner licensed in the state of New Jersey, together with such other professionals licensed in the state of New Jersey (e.g., traffic engineer) as may be appropriate. In addition, the party requesting the amendment shall establish an escrow account with the Borough adequate to allow the Borough and/or Planning Board to use the services of a professional planner and other necessary professionals licensed in the state of New Jersey to identify, review and/or prepare further amendments that might be needed, together with any and all necessary documentation related thereto.

# Appendix: Subsections 184-119D through I of the Land Use Ordinance of the Borough of Fanwood

## D. Architectural Regulations

The architectural regulations in this section apply to all new development and renovations within the CC-W, CC-C and CC-E Districts, unless otherwise specified in these Regulations. Big box or retail chain architecture is not permitted in any District.

### (1) Architectural style.

- (a) **CC-W District.** New buildings, or renovations to existing buildings, may be less eclectic than the Victorian styles employed in the CC-C District. Examples of appropriate architecture include Craftsman, Mission, Shingle, or other style approved by the Board. Architecture may also incorporate a modern or industrial aesthetic, but should avoid harsh, bulky structures. Buildings should incorporate as much light as possible, and be built with interior flexibility, so they can be adapted for reuse over time. Buildings are of a sturdy structure with simple, straightforward volume, and may feature natural colors, mixed materials, clean lines, overhanging eaves, cantilevered balconies, ample windows, awnings or canopies. A mixture of building materials- glass, stones, bricks, wood- can contribute to an organic feel.

*Examples of architectural styles appropriate for the CC-W District.*



- (b) **CC-C District.** New buildings shall be designed to reflect a late Victorian period character that typify the historic Fanwood Train Station and the existing Victorian-style architecture employed in the Block 64 Redevelopment Area. Renovations to existing buildings should complement this architectural style. The objective is to create an uninterrupted retail frontage along South Avenue and Martine Avenue that reflects a “village downtown” environment.



*The historic Fanwood Train Station and existing development along South Avenue employ elements of Victorian architecture.*



- (c) **CC-E District.** New buildings or renovations to existing structures should take on a residential look, having peaked and gabled roofs. Buildings closest to the CC-C District shall incorporate design elements that reflect the Victorian architecture of the CC-C District.

*Examples of existing exemplary architecture in the CC-E District. Buildings are single use or mixed-use and have architectural features representative of residential housing in Fanwood. Roofs are peaked and contain dormers. There is ample architectural detailing through windows, color, material changes, and trim, for example.*



*The buildings in the CC-E District shall feature residential styling that include elements such as color and material contrasting, peaked roof variations, window mullions, and significant articulation, as well as attention to landscaping and hardscaping.*



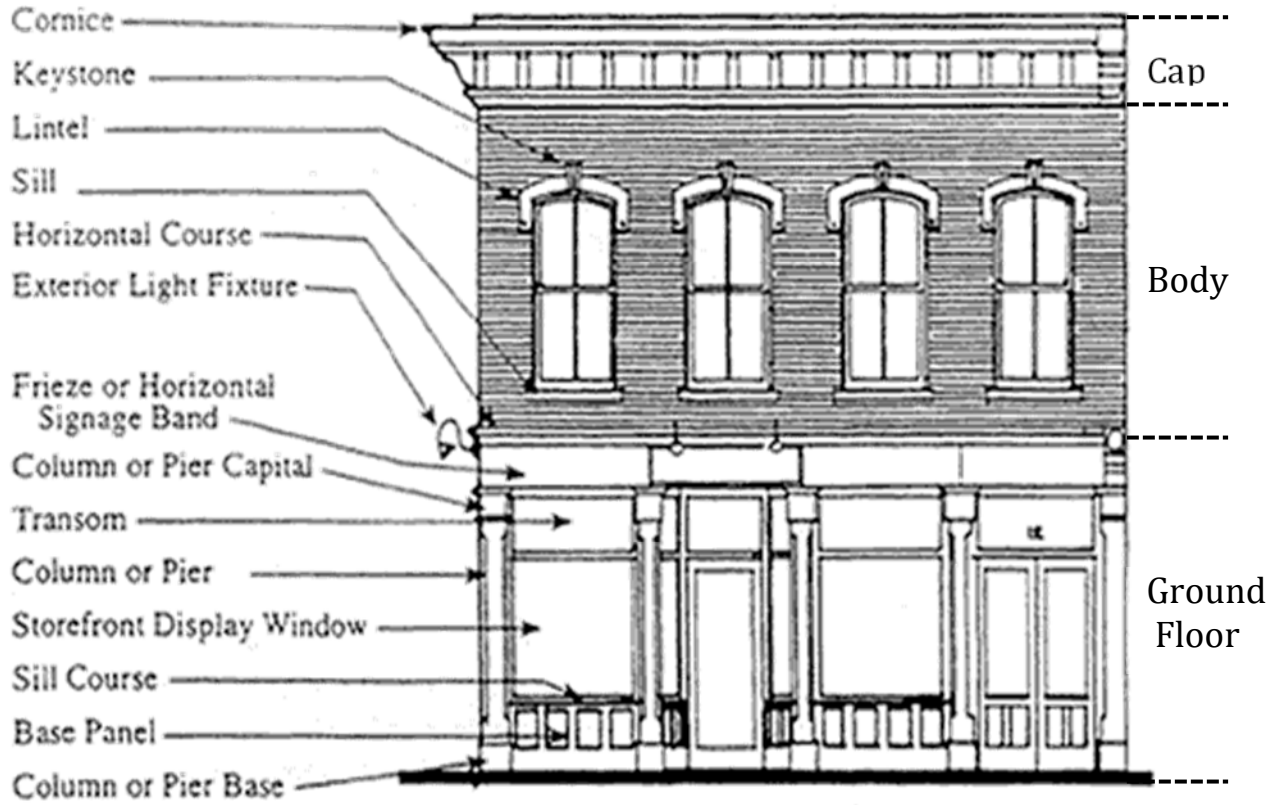
(2) **Building orientation.**

- (a) Buildings shall be clustered to encourage pedestrians to walk between buildings and properties. Buildings should create a continuity of building façades to form a street wall along the streetscape.
- (b) Primary building entrances shall be oriented towards streets, parks and plazas. Building entrances shall be directly accessible from the sidewalk.

(3) **General Articulation.** Articulation of the facade is necessary to avoid monotonous buildings, including horizontal and vertical projections and recessions, ornamentation and other design elements to make it visually stimulating.

- (a) All new buildings and the alteration of buildings shall be designed with particular consideration to the quality and character of the façade of the building, particularly at ground or street level.
- (b) Long, blank, windowless, monotonous, uninterrupted walls or roof planes are not permitted.
- (c) The architectural treatment of a façade shall be completely continued around all street- or parking lot-facing facades of a building. All sides of a building shall be architecturally designed so as to be consistent with regard to style, materials, colors and details.
- (d) **Façade divisions.** All visibly exposed sides of a building shall have a clearly defined ground floor, body and cap.
  - i. **Ground floor.** The ground floor façade connects the building to the adjoining pedestrian area. It should have a greater height and a façade treatment that is distinct from the floors above, and can include design details such as recessed entry areas, covered outdoor areas, alcoves, awnings, etc. The ground floor can be further edged with horizontal banding or a cornice line.
  - ii. **Body.** The body or middle of the building is the floor or floors between the ground floor and the building cap.
  - iii. **Cap.** Design details which terminate or “cap” the top of a building shall be used. The cap should feature either pitched roofs or articulated cornices or parapets, and a change of color and material.

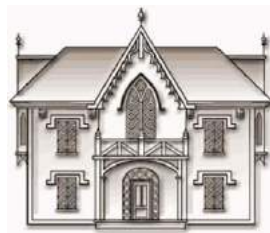
*Architectural Features*



*Residential Architectural Styles*



Victorian



Gothic Revival



Shingle



Italianate



Second Empire



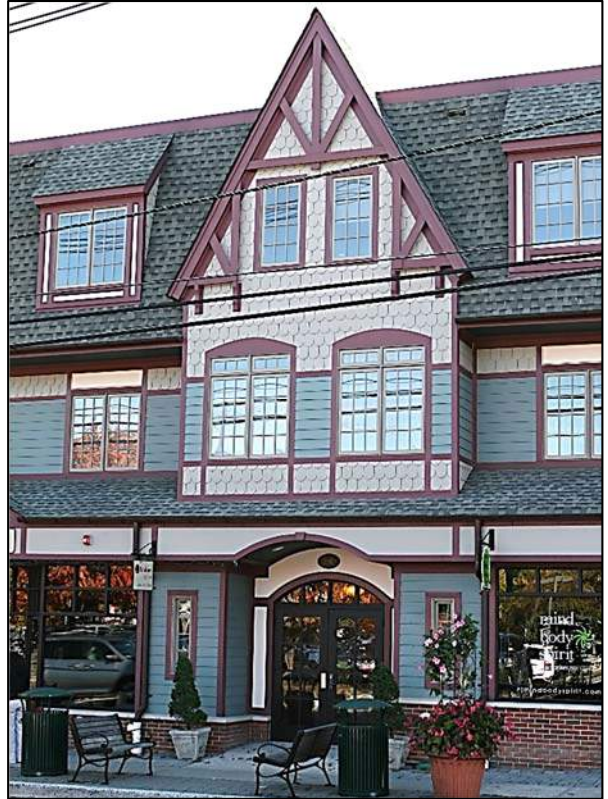
Craftsman

- (4) **Vertical Projections.** Vertical building wall offsets, including both projections and recesses, shall be provided along any building wall measuring greater than twenty (20) feet in width to provide architectural interest and variety to the massing of a building. The minimum projection or depth of any vertical offset shall not be less than six (6) inches. Vertical offsets can include columns, piers, pilasters, projecting bays, and changes in façade material.
- (5) **Horizontal projections.** Facades should be articulated with horizontal divisions to differentiate the buildings base, cap and wall. Horizontal offsets may also include awnings, string course, horizontal break bands, frieze board, cornice, roof lines, window sills, balconies, or other architectural ornamentation.
- (6) **Building variation.** Along a street frontage, buildings shall vary in terms of their footprint, architectural elevations, roof heights, roof orientations, and front entrance locations to create architectural variety.
- (7) **Alignment of features.** The alignment and repetition of certain façade elements from one building to the next creates visual continuity, while affording architectural variety. Where practical, consideration shall be given to aligning horizontal elements with adjacent buildings, including: top and bottom height of first floor display windows, storefront windows, building kickplate, window heights, sign band, parapet and cornice line, window sills on upper floors.
- (8) **Infill projects.** Proposed infill projects should complement and strengthen the surrounding neighborhood and reinforce desirable community design patterns and the architectural features in these Standards.

(9) **Materials.**

- (a) Materials shall consist of the following, individually or in combination: brick, fiber-cement lap siding, fiber-cement shingles, clapboard or synthetic alternative (Hardie Board), mosaic tile, wood or architectural metal.
- (b) Exterior Insulated Finishing Systems (IEFS) and vinyl siding are not permitted.
- (c) Façade patterns in the CC-C District shall consist of those particular to Victorian Architecture, including fish scale, diamond and staggered designs, particularly to highlight architectural details, such as a gabled roof wall. Trim ornamentation shall consist of wood, composite or polyurethane millwork, stone, or fiberglass. Victorian design elements can include cut ornamentation, projected bay, towers and/or turrets, horizontal banding, brackets, trim, bargeboard, frieze board, and exterior wall pattern.

*Articulated street-facing and parking-lot facing facades of new Victorian-styled developed in Fanwood.*



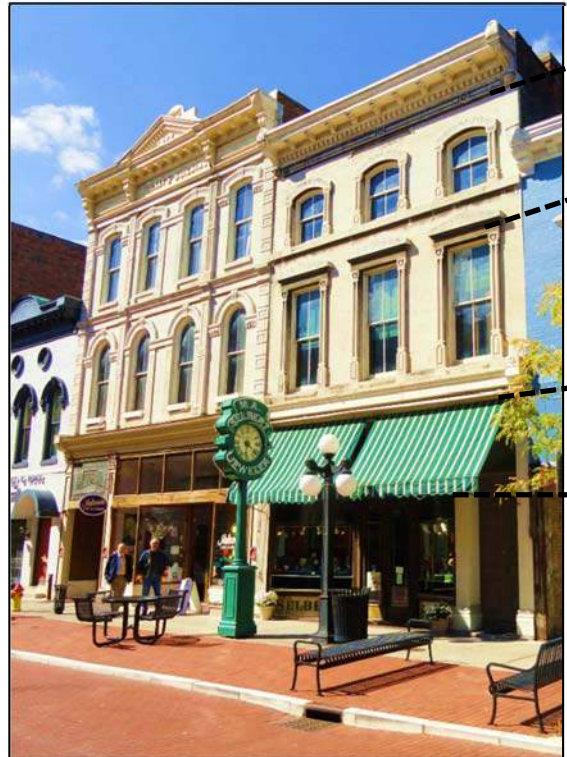
- (10) **Colors.** Building facades, inclusive of all accent elements and signs, shall be limited to a maximum of four (4) colors. Treating the surface wall and trim with different, but coordinated colors, is recommended. Where a Victorian Architecture is employed, the structure shall have a subtle color and a contrasting color shall be used for the building's accents.



*Vertical articulation using column piers and windows at equally spaced intervals.*



*Horizontal alignment of features with adjacent buildings (window sills, cornice lines and trim, awning and transom)*



*Varied elevations and roof types*



(11) **Building lighting.** Light fixtures attached to the exterior of a building are encouraged, and should be architecturally compatible with the style, material, and colors of the building. The use of creative lighting to highlight building facades are encouraged. For retail stores, storefronts should be illuminated to highlight merchandise in the display window. Low-pressure sodium or mercury lighting is prohibited.

*Light fixtures to highlight signage, façade elements and the pedestrian realm.*

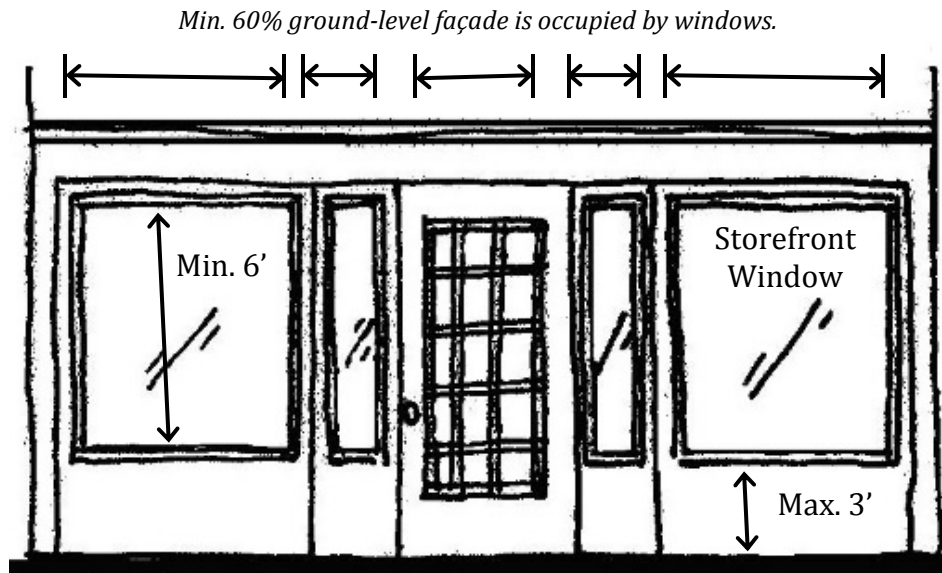


(12) **Windows and transparency.**

- (a) At least 30% of any street-facing wall surface must have a clear glazing.
- (b) Ground floor uses shall have large pane display windows or storefront glass along the street frontage that shall occupy at least 60% of the first floor façade in the CC-C District and at least 40% of the first floor faced in the CC-W and CC-E Districts. The first floor façade is the area bounded the side edges of the building and the plane coincident with the internal floor of the building and internal ceiling of the building.
- (c) Window glass on the ground floor shall provide unencumbered views to window displays and advertised retail goods and services. Mirrored glass, tinted glass, and glass blocks are prohibited.
- (d) Ground floor windows facing the street shall be a minimum of six (6) feet in height and the bottom of which shall be no more than three (3) feet above grade. Storefront windows may be either large, single pane or multiple smaller panes, separated by mullions.
- (e) All windows on a building having Victorian Architecture shall be vertically proportioned in that they are taller than they are wider. Windows on the upper stories may have multiple panes separated mullians. Round-top windows, dormer windows, bay windows, and transoms above display windows are characteristic of Victorian architecture.
- (f) Upper story windows shall be detailed with architectural elements, such as projecting sills, molded surrounds and/or lintels.



- (g) Exterior security grates are not permitted on windows.
- (h) Windows and doors should be selected with their energy conservation value in mind to maximize energy savings.
- (i) Where unique use or occupancy requirements preclude the addition of windows, such as theaters or parking structures, exterior walls shall be designed to provide architectural relief or shall be screened by landscaping and pedestrian amenities, such as trellises, benches or shade structures.



**(13) Building entrances.**

- (a) Entrances to a building shall be clearly identifiable and directly accessible from the sidewalk.
- (b) Entrances shall be defined and articulated by architectural elements such as canopies or porticos, overhangs, awnings, recessing or projections, lintels, moldings, pilasters, columns, trim or other elements that are architecturally compatible with the building style and can help establish the entrance's prominence. Projecting elements add three-dimensional interest to facades and enhance the sense of the entry into a building.
- (c) Recommended entranceway treatments include decorative light fixtures, ornamental ceiling treatments, attractive door handles, hinges and other hardware.
- (d) Entrances shall be defined with exterior site or sidewalk features such as planters, decorative paving, or outdoor patios where such features do not interfere with pedestrian traffic.
- (e) Entrances should complement, not overpower the building façade.



(14) **Awnings and canopies.** Awnings are encouraged to provide shade, relief from rain, and to enhance a building's façade.

- (a) Awnings and canopies shall be architecturally compatible with the building and overall building color scheme.
- (b) Awnings may be fixed or retractable. Canopies are permanent elements of the façade.
- (c) Awnings shall be made of waterproof canvas, cloth, metal or equivalent and kept in good order and repair. Awnings made of vinyl or plastic are not permitted. Canopies are constructed of stone, metal, wood or glass.
- (d) Awnings and canopies shall not extend more than 3 feet from the building façade and shall not be less than 8 feet above ground.
- (e) Internally illuminated or backlit awnings are not permitted.
- (f) Awnings shall not conceal architectural elements such as cornices, columns, pilasters or other trim details.
- (g) Awnings shall be placed to align with existing awnings on the same side of the street.

*Awnings provide protection from weather elements and enhance the building façade.*



*Recessed entryway flanked with plantings, façade piers and light fixtures. Display windows, awnings, and transoms enforce entrance symmetry.*



(15) **Balconies.** Balconies shall be a minimum of six (6) feet deep and five (5) feet wide to create a functional outdoor space for upper story uses. A maximum of 40% of the front and corner side facades of the building above the first floor, as calculated separately, may be covered by balconies.

(16) **Roofs and Building Height.**

- (a) The type, shape, pitch, texture and color of a roof shall be architecturally compatible with the style, materials, colors and detailing of the building.
- (b) Roof lines may vary in type and height to provide an interesting view from the street.
- (c) Pitched roofs are characteristic of Victorian Architecture and are required in the CC-C District. Pitched roofs can include gable, cross gable, hip and mansard, and shall have a minimum pitch of 8:12.
- (d) Roofline offsets shall be provided along any street-facing or parking lot-facing roof measuring more than 30 feet in length. Roofline offsets may include dormers, chimneys, turrets, towers, cupolas, and other elements that are compatible with the style, materials, color and details of the building.
- (e) Roof materials on Victorian style buildings shall be either slate, shingles or metal. Shingles may mimic a slate texture, color and shape. Roof eaves with ornamental brackets shall be considered to reflect Victorian architecture, where appropriate.
- (f) Towers should be limited to corner buildings, adjacent to public spaces and street termini.
- (g) All mechanical equipment, HVAC systems, exhaust pipes and stacks, stairways, satellite dishes, telecommunications receiving devices, or other rooftop elements shall be screened or otherwise treated to be inconspicuous as viewed from the street. Such elements shall be set back from the roof edge at least one (1) foot for each one (1) foot of height.

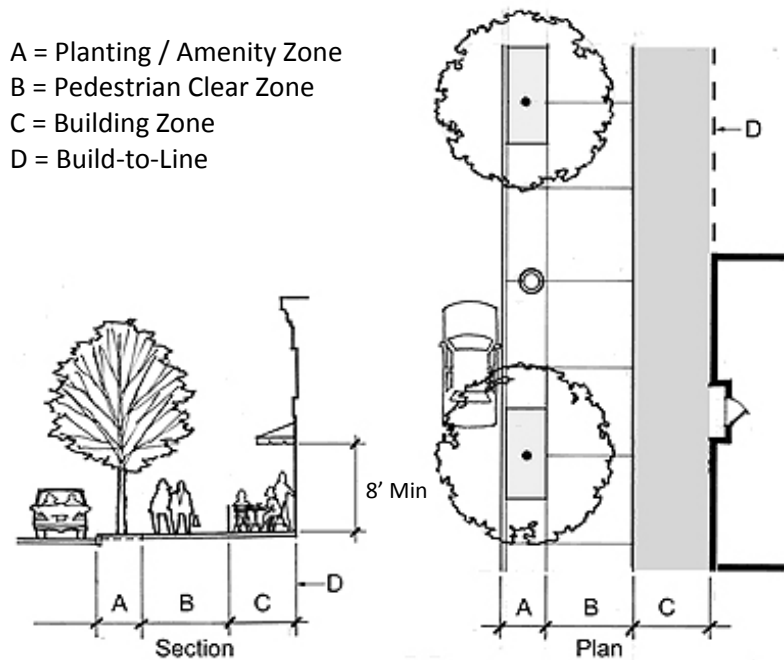


*A turret's 3D projection and roof dome emphasize a corner building.*

## E. Streetscape design

- (1) **Sidewalk Zones.** Sidewalks shall be comprised of the following zones.
  - (a) **Building Zone.** Where space permits, the area nearest the façade may accommodate planters, sidewalk sale merchandise, seating areas, outdoor café use, movable container plantings and/or permanently installed foundation plantings. Sidewalks within the CC-W and CC-E Districts may be built without a building zone.
  - (b) **Pedestrian Clear Zone.** The pedestrian clear zone shall be a minimum of five (5) feet to permit safe, unimpeded circulation routes.
  - (c) **Planting / Amenity Zone.** The planting zone extends a minimum of three (3) feet from the curb line to accommodate street trees, pole-mounted site features, trash receptacles, parking meters, sidewalk furniture, signs, and other sidewalk amenities. In the CC-W and CC-E Districts this area shall be a tree lawn for grass, turf or groundcover. For tree lawn areas less than 2 feet wide, pavers (brick, flagstone or concrete) may be considered.

*Sidewalk Zones (Section & Plan View)*



- (2) **Sidewalk widths.** Sidewalks shall be a minimum of eight (8) feet wide along South Avenue within the CC-C District and a minimum of five (5) feet within the CC-W and CC-E Districts. Increased sidewalk widths may be appropriate in highly trafficked areas.

(3) **Paving.**

(a) The standard 5 foot by 5 foot concrete pattern sidewalk may be utilized in the CC-W and CC-E Districts. Substantial planting areas shall be located to either side of the sidewalk.

(b) Within the CC-C District, the prescribed sidewalk shall match the existing decorative sidewalk employed along South Avenue and Martine Avenue within the Block 64 Redevelopment Area.

(4) **Street trees.** Street trees shall be provided in accordance with Section 184-100 and with these provisions. Where tree lawns do not exist, tree grates or pavers are recommended to protect street tree roots and pedestrians.

(5) **Street tree spacing.** Spacing of street trees shall be based on recognized planting guidelines for the selected species type. Generally, large, maturing trees shall be planted a minimum of 40 feet and a maximum of 50 feet on center. Small and medium maturing trees shall be planted a minimum of 10 feet and a maximum of 30 feet on center.

(6) **Amenities.** Streetscape amenities such as patio and seating areas, outdoor cafés, water features, art work or sculpture, clock towers, pedestrian plazas, sandwich board signs, or other features are highly encouraged. In addition to benches, the provision of low walls, planters, and stairs is encouraged to provide outdoor seating. Merchandise for sale may be displayed outdoors, where items do not block the pedestrian clear area.

*Standard 5' x 5' concrete sidewalk with vegetation to either side, appropriate for the CC-W and CC-E Districts.*



*Examples of decorative sidewalk patterns*



## F. Off-street parking and circulation

- (1) **Driveways.** Vehicular entrances to parking lots shall be kept to a minimum along South Avenue and Martine Avenue. One driveway per lot is permitted. Driveway access from a side or secondary street is preferred. Shared driveways are encouraged. On corner lots, vehicle access should not be from South Avenue or Martine Avenue, where feasible.
- (2) **Curbs.** Curbs along public right-of-ways shall be poured-in-place concrete or other masonry material such as Belgian block. Curbs for off-street parking areas shall be Belgian block.
- (3) **Parking design.** Parking lots shall be designed to have a positive visual impact with generous landscaping, sizable trees, decorative walls, raised walkways, changes in paving materials, pedestrian-scale lighting, and other features. Parking that requires vehicles to back out onto a public right-of-way is prohibited.
- (4) **Screening.** Surface parking lots must be fully screened from the front property line by building frontages. Where there are gaps between buildings, or where parking facilities are visible from a right-of-way, ornamental fencing, decorative walls or landscaped hedges at least three (3) feet high shall continue the street wall.
- (5) **Parking lot plantings.**
  - (a) **Interior plantings.** Internal landscaped planting islands shall be installed to provide shade and beautification. A minimum of 1 shade tree per 10 parking spaces shall be provided within the interior of a parking lot.
  - (b) **Perimeter plantings.** Shade trees, shrubbery and other landscaping shall also

*Outdoor café space, seating, planters, large trees, awnings, signage, etc. enhance the sidewalk area*



*Merchandise display on sidewalk*



be provided along the perimeter of a parking lot to provide shade, buffering and screening.

- (6) **Structured parking.** Structured parking and underground parking are permitted in the CC Districts and is particularly encouraged in the CC-C District to better utilize land that is close to the transit station and downtown. Structured parking shall be screened from view of any street by wrapping the ground floor with retail and other street-facing uses.
- (7) **Drive-through establishments.** Drive through facilities are not permitted, except for banks in the CC-E District. Drive through facilities must be located to the rear of the building and be screened with planting and/or architectural walls to minimize their visibility.
- (8) **Loading requirements.** Loading areas shall be placed to the rear or side of buildings and, to the extent possible, shall not be visible from a public street. Loading areas shall be designed so that trucks can circulate on site and do not back out onto a public street. Screening and landscaping shall be provided to minimize their visibility and shall be achieved through walls, fences and landscaping that is a minimum of five (5) feet tall.
- (9) **Required parking.**
  - (a) **Number of parking spaces.** The number of parking spaces required shall be provided in accordance with the Borough's Parking Requirements (184-160), except as noted below, where space calculations for certain uses are based on the *Parking Study for Façade Upgrade Guidebook and Shared Parking Strategy*, prepared by Maser Consulting, P.A. dated August 30, 2012:
    - i. **Retail and service:** 3 spaces per 1,000 sq. ft. GFA
    - ii. **Office:** 4 spaces per 1,000 sq. ft. GFA
    - iii. **Restaurant, bar, and tavern:** 3 spaces per 1,000 sq. ft. GFA
    - iv. **Multi-family residential:** 1.5 per dwelling unit.
    - v. **Mixed-use:** A shared parking approach to the provision of parking shall be submitted.
  - (b) **Shared parking.** Shared parking is encouraged in all districts. Where a shared parking approach between multiple sites and/or uses is proposed, the Planning Board may reduce the required parking where the applicant can demonstrate that the uses have differing time-of-day peak demands. As a condition of approval of a shared parking strategy, an easement, deed restriction or other legal instrument shall be provided that is satisfactory to the Borough attorney.
  - (c) **Bike parking.** Bike parking shall be provided in all CC Districts to make cycling a more convenient transportation option. Bicycle parking facilities shall adhere to the requirements below. Buildings containing multiple tenants may satisfy their individual requirements by providing a single bike rack in a centrally located area.

- i. **Multi-family residential:** 1 space per unit, located within the interior of each individual unit, or within a secured common area within the building.
- ii. **Non-residential:** 2 spaces per 5,000 sq. ft. GFA, located on the site within 50 feet of the principal building entrance and within a highly visible, well-lighted area that does not impede pedestrian or vehicular traffic.
- iii. **Mixed-use:** both interior and exterior bike parking spaces as per the residential and non-residential parking requirements above.
- iv. **Parking garage:** 1 space per 10 vehicles.

(10) **Pedestrian Circulation.** Pedestrian walkways and entryways shall be provided for all uses. They shall be prominent, well-lit and separate from vehicular entrances.

*Examples of indoor and outdoor bike parking*



**G. Site landscaping**

- (1) These landscaping requirements shall supplement Section 108-104 of the Borough's Land Use Code.
- (2) All unpaved areas, open spaces and plazas shall be attractively planted and landscaped with lawns, trees and shrubs.
- (3) Plants in boxed, clay, wood, or other decorative containers are encouraged to enhance walkways and building entrances.
- (4) Planting boxes on window sills or hanging plants are encouraged.
- (5) The use of vines and climbing plants on buildings, trellises and perimeter walls is encouraged, particularly on industrial buildings to improve their appearance from the street.

- (6) Plantings in the front yard or right-of-way shall not obscure architectural building details and shall respect views to and from streets, porches, walks and public open spaces.
- (7) Loading areas shall be sufficiently landscaped to screen the view of the loading area and vehicles from any public right-of-way and residential property.
- (8) Containers and compactors for trash and recycling shall be attractively and compatibly designed and fully enclosed or screened by permanent materials and shall be secured and accessible.

#### **H. Fences and walls.**

- (1) Fences and walls shall be provided in accordance with Section 184-108 of the Land Use Code and with these provisions.
- (2) Fencing may be steel, wrought-iron, or any other fence type approved by the Planning Board. Chain link fence is not permitted. Wood walls or fences are not permitted along South Avenue.
- (3) No wall or fence may extend continuously for more than 50 feet without articulation. Articulation may include decorative columns, changes in material or texture, offsets (projections and recessions), landscape pockets, or similar features.





*Examples of planter boxes attached to window sills on upper and lower floors.*



*Use of vines, trellis work and vegetation can dramatically improve the appearance of bldgs..*



*Planters at building entrances*



## I. Signs

- (1) These sign requirements shall supplement Article XVII of the Borough's Land Use Code. Where there is a conflict between these requirements and Article XVII, these requirements shall prevail.
- (2) Signs should be clear, informative and weather well.
- (3) **Wall signs.**
  - (a) The type, size, shape, orientation, lettering, color, materials and illumination of wall signs must be designed to fit within the architecture of the building.
  - (b) Signage design is encouraged to be creative and unique to the establishment. The type and design of signs should vary among individual businesses along a street frontage. To the extent feasible, wall signs should align in height and scale with other signs on the block to maintain an alignment of horizontal and vertical façade features.
  - (c) Signs should be positioned to emphasize special shapes or details of the façade, to draw attention to the shop entrance, or to emphasize a window display. They should not obscure important architectural building details.
  - (d) Roof mounted signs are prohibited.
  - (e) A street-facing façade may have up to two (2) signs, including a projecting sign, a flush-mounted sign or a window sign.



*Façade sign with external lighting located above display windows and below transom*



*Façade sign above display window and address number on façade*



*Example of a creative façade sign*

- i. **Wall mounted.** Wall-mounted signs can be attached directly to the building within a sign band or frieze. In the absence of a sign band or frieze, the bottom of the sign may not be positioned more than 1 foot above the top of the display window of the establishment.
- ii. **Awning and signs.** A sign can be professionally painted, screened, applied or sewn on the front panel or valence area of an awning. Letters should be sized to allow for an inch of open space at both the top and bottom of the valence and lettering should extend no more than 75% of the width of an awning.
- iii. **Projecting/blade.** A projecting “shingle” or “blade” sign may be permitted perpendicular to the street over the sidewalk to identify the name or logo of the business to which the sign is attached. The maximum size of a shingle sign is 3 square feet, except that signs of a shape other than a square or rectangle may be up to 5 square feet in area. The bottom edge of this type of sign shall be at least 8 feet above grade. A projecting sign may be mounted on an ornamental bracket and may only be illuminated by directed external lighting.
- iv. **Window sign.** Professional lettering may be provided on a glass display window.

*Example of a business with effective awning sign along valence and window sign.*



*Examples of projecting signs*



- (f) Signs should be lit at night using external incandescent spotlights or gooseneck lighting. Backlit or internally lit signs are prohibited. Unshielded spotlights, neon, moving and flashing signs are prohibited.
- (g) Businesses located on an upper floor may be identified by wall mounted or blade signs on the first floor.

- (4) **Sandwich board signs.** Portable sandwich board signs are permitted in accordance with §184-157.D.
- (5) **Freestanding signs.** Low mounted freestanding, hanging post or monument style signs are only permitted in the CC-W and CC-E Districts. The maximum surface area for each side of a freestanding sign is 25 square feet, and the maximum height above grade is five (5) feet. A freestanding sign must be setback at least 10 feet from the front property line and 20 feet from any other property line. Internal illumination is prohibited.

*Examples of creative, low-height freestanding signs appropriate for the CC-W and CC-E Districts.*



## Definitions

Artisanal Workshop -- Shops of special trade including the small-scale manufacturing, compounding, assembly processing, packaging or similar treatment of such products as: baked goods, candy, ceramics, pottery, china, weaving and other textile arts, painting, woodworking and other artistic endeavors and similar trades. Retail sales on the premises of products made on the premises are required. Artisanal workshops shall not exhibit any exterior noise, smell, glare or dust from manufacturing.

Base panel – The area of a building between the first floor windows and the ground.

Bracket – A projection from a vertical surface providing support under cornices, balconies, window frames, etc. They can be structural or decorative.

Cornice – A projecting ornamental molding along the top of a building or wall.

Cornice Line -- A continuous horizontal cornice feature that provides a vertical break within the facade and/or defines the roofline along the top of the building.

Craftsman Industrial – See Artisanal Workshop

Eave – The lower part of a sloping roof projecting beyond a wall

Frontage -- The area between a building facade and the vehicular lanes, inclusive of its built and planted components. Frontage includes private frontage and public frontage.

Frieze board – a long, horizontal decorative band, usually located under the eaves

Front lot line coverage – The width of the building's primary façade measured at the maximum permitted front setback as a

percentage of the width of the front street property line.

Gable – The upper, usually triangular, part of a wall below the end of a roof with two sloping sides.

Gateway -- A point at which a motorist or pedestrian gains a sense of arrival in a particular district, neighborhood or part of a municipality. This impression can be imparted through such things as signs, monuments, landscaping, change in development character, or a natural feature.

Hardscape -- Inanimate elements of landscaping. Examples include walls, patios, pavers, paths, boulders, stones, decks, and fountains.

Lintel – A horizontal beam or stone that spans an opening

Live/Work Unit - A single unit (e.g., studio, loft, or one bedroom) consisting of both a commercial and a residential component that is occupied by the same tenant. No portion of the live/work unit may be rented or sold separately. Residential areas are permitted above the commercial component, to the side or in back of the business component, provided that there is internal access between the residential and commercial space.

Rehabilitation Center - An establishment providing short-term care, treatment and/or rehabilitation services for persons recovering from surgery, debilitation or serious illness that are transitioning from an in-patient hospital status to independent living.

Muntin -- A small molding or bar that separates the individual panes of a window. Also known as a mullion.

Personal services -- Specialized services purchased frequently by the consumer, including barbershops, beauty shops, spas, laundry cleaning, tailoring, shoe repair, and other similar establishments.

Pilaster – A column of rectangular shape with a base and capital, applied ornamentation, not structural support.

Shared Parking -- Joint utilization of a parking area for more than one use, either on site or between nearby properties through a dedicated arrangement, in order to fulfill their individual parking requirements because their prime operational hours do not overlap.

Sill level – A string course that is continuous with the window sills or immediately below them.

String course – A projecting horizontal band across an exterior wall of a building.

Story – That portion of a building included between the surface of any floor and the surface of the floor next above it, or if there is no floor

above it, then the space between the floor and the ceiling next above it.

Story, half – Habitable space under a sloping roof that has the line of intersection of the roof and the wall face not more than 3 feet above the floor level and in which the possible floor area with head room of 6.5 feet or more occupies no more than 50% of the total floor area of story directly beneath.

Street wall -- A wall or portion of a wall of a building that faces a street, and generally refers to how and where several buildings line up to define a walking environment.

Transom window – A window above a door or window.

Turret – A circular or multi-sided small tower that projects vertically from the wall of a building, commonly associated with Victorian and Queen Anne architectural styles. A turret may contain a staircase if it projects higher than the building, but may also be part of a room extension.